

WARD: Hartcliffe & Withywood

SITE ADDRESS: (Bathurst Basin Bridge Commercial Road) Land Between The A370 Long Ashton Bypass In North Somerset And Cater Road Roundabout Cater Road Bristol

APPLICATION NO: 22/05943/X Variation/Deletion of a Condition

DETERMINATION DEADLINE: 16 March 2023

DEADLINE:
Application for the removal of conditions 4, 10 and 13 following grant of planning application 16/05853/X for the variation of condition number 18 - Phase 1. for planning permission - 13/05648/FB.

RECOMMENDATION: GRANT subject to Planning Agreement

APPLICANT: Bristol City Council
100 Temple Street
Bristol
BS1 6AG

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



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UPDATE FOLLOWING WITHDRAWAL FROM COMMITTEE

This application was due to be considered by Development Control Committee B at a meeting on 10 May 2023 (the May Committee Meeting), however it was withdrawn from consideration prior to the meeting at the request of the applicant. The request for withdrawal was accepted by the Committee Chair. It is understood that the applicant, alongside the Mayor's office, wished to consider the evidence submitted in public statements to the May Committee Meeting and whether the fire at Underfall Yard affected the scheme.

The applicant has asked that the LPA determine the scheme as submitted to the May Committee Meeting. No amendments have been made to the application in the time between the May Committee Meeting and this Committee Meeting.

Outside of the planning process, the Highway Authority has used its Permitted Development Rights to informally create additional width for pedestrians. These works do not require planning permission and as such, do not form part of the proposals under consideration in this planning application.

In September 2023, Avon Crescent was reopened to traffic following the expiration of Temporary Traffic Regulation Orders which were put in place to support roadworks on Cumberland Road.

SITE DESCRIPTION AND BACKGROUND

In 2014, planning permission was granted for a revised section of the Ashton Vale to Temple Meads (AVTM) section of MetroBus, from Avon Crescent, along Cumberland Road to Redcliff Hill (application ref: 13/05648/FB).

This planning permission comprised of a new junction with Cumberland Road, a new bridge at Bathurst Basin, flood protection measures, demolition and reconstruction of walls, realignment of highway, crossings, traffic signals and temporary construction areas, bus stops and shelters.

In 2016, an application (ref. 16/05853/X) was approved to vary an approved plan under section 73 of the Town and Country Planning Act 1990 on planning permission 13/05648/FB to allow for the position of the proposed new bridge at Bathurst Basin to be amended.

Avon Crescent

Avon Crescent is characterised by a row of Grade II Listed Georgian houses, located along the eastern side of the road, fronting onto a footway and the highway. The western side of Avon Crescent is comprised of the retaining structure for Cumberland Road and a brick substation building with parallel parking bays along the highway. A stepped access through the retaining wall provides pedestrian access from Cumberland Road to the southern end of Avon Crescent.

Included within the planning permission (ref. 16/05853/X) for AVTM, is a phase of development associated with MetroBus at Avon Crescent which is yet to be implemented. This is comprised of:

- One-way exit from Avon Crescent to Cumberland Road – closing Avon Crescent to normal motorised traffic entering from Cumberland Road.
- Contraflow cycleway access from Cumberland Road to Avon Crescent.
- Realigned retaining wall between Cumberland Road and Avon Crescent.
- Reconfigured junction between Avon Crescent and McAdam Way including creation of pedestrian footway areas.

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- Echelon parking layout for parking bays on west side of Avon Crescent.
- 'Shared space' highway surface treatment, comprised of pennant stone paving / conservation stone paving, hard and soft landscaping.

These improvements are detailed on previously approved plans 201749-PA-202 and 201749-PA-203 on the extant consent ref: 13/05648/FB.

Extant permission ref. 16/05853/X sought to vary condition 18 to realign the position of the new single span bridge constructed alongside the existing Bathurst Bridge. This part of the scheme has been implemented and Avon Crescent was not affected by that 2016 application.

In June 2018, application ref. 18/02968/X was submitted to vary condition 15 of permission ref. 16/05853/X to provide an alternate scheme of works for Avon Crescent. These details are set out on drawing RS15012 – SK03A.

This scheme was refused in February 2019 on the basis that:

The proposed development would be harmful to conditions of highway safety, especially for pedestrians, contrary to Policy BCS10 (Transport and Access Improvements) and Policy DM23 (Transport Development Management), which seek design developments to provide safe and adequate access to all road users.

Between September 2021 and September 2023, a Temporary Traffic Regulation Order (TTRO) was in place for the closure of Avon Crescent to vehicular traffic to support roadworks on Cumberland Road. This TTRO has expired and the road has reopened to traffic.

PLANNING HISTORY

18/02968/X - Application for variation of a condition No. 15 (List of Approved Plans) following grant of planning permission 16/05853/X.
REFUSED - 05/02/2019.

16/05853/X – Application of variation of condition number 18 – Phase 1. For planning permission 13/05648/FB
GRANTED – 02/02/2017.

16/05418/NMA – Application for a non-material amendment for removal of the Cumberland Road outbound bus lane from proposals.
AGREED – 02/11/2016.

13/05648/FB – Revision to the route of the rapid transit scheme authorised by the Ashton Vale to Temple Meads and Bristol City Centre Rapid Transit Order (the Order). The development comprises construction of a new junction with Cumberland Road, a new bridge at Bathurst Basin, floor protection measures, demolition and reconstruction of walls, realignment of highway, crossings, traffic signals and temporary construction areas, bus stops and shelter.
GRANTED – 18/03/2014.

The Ashton Vale to Temple Meads and Bristol City Centre Rapid Transit Order was made on 19/12/2013 under the Transport and Works Act 1992.

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NB – since grant of planning permission 13/05648/FB, numerous applications to discharge conditions associated with construction phases of this planning permission have been submitted and approved.

Reports about non-compliance with planning conditions have been made since the refusal of application ref. 18/02968/X by interested parties. These reports have been considered by the Planning Enforcement team within the Local Planning Authority (a decision not to register a formal planning enforcement investigation was recorded under planning enforcement case reference 20/30331/BCN on 25.9.2020). More recent reports have also been received and the position advanced is that the application the subject of this report needs to be decided before a review of any outstanding planning requirements can be undertaken (such as compliance with planning conditions).

APPLICATION

This application seeks to remove conditions 4, 10 and 13 from planning permission ref. 16/05853/X under section 73 of the Town and Country Planning Act 1990. These conditions are written out in full for ease of reference below.

Condition 4

“Submission and approval of replacement tree planting scheme

No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority details for 55 replacement trees to be planted in the approved locations, or alternative locations to be agreed by the Local Planning Authority.

The details shall include the species, stock size, staking and guarding and establishment arrangements of each tree as well as a programme of works for the planting of the trees.

The approved scheme shall be implemented so that planting can be carried out during the first planting season following the commencement of the AVTM MetroBus service. The trees shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To ensure that the development provides adequate mitigation for the loss of the trees on the site and complies with the Bristol Tree replacement Standards.”

Condition 10

“Submission of full design details including variations

The following aspects of the scheme numbered i-v shall be submitted as a coordinated submission in relation to each phase of work and be approved in writing by the Local Planning Authority before the works approved in that work phase are begun;

i. all hard landscaping (including paving, surfaces, edge details and the retention and reinstatement of existing pennant stone kerbs, cast iron kerb edges and stone sett channels). ii. soft landscaping showing existing planting to be retained and new planting (including species, planting sizes,

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planting densities, planting soils, planting pits and staking, root barrier to enable planting to be carried out in close proximity to underground services, flood retention ponds, ground and earth modelling).
iii. street furniture and equipment, (including, signals, control equipment and signage).
iv. street lighting (including a lighting level contour plan to assess light spill impacts on ecology).
v. bus stop infrastructure.

In drawings submitted to satisfy this condition the following amendments to the drawings submitted with the application shall be made:

- 1. Parking layout within Avon Crescent to ensure that an appropriate means of access is retained to Underfall Yard*
- 2. Review of crossing points in Avon Crescent so that they observe pedestrian desire lines, and associated landscaping.*
- 3. Insertion of a dropped kerb on Cumberland Road to west of the new bridge at Bathurst Basin to provide access onto the shared cycleway/footpath.*

Unless alternative times for implementation are otherwise agreed in writing by the Local Planning Authority, the scheme shall be implemented in accordance with the plans approved under this condition prior to the commencement of the AVTM MetroBus service with the exception that planting may be carried out no later than during the first planting season following the commencement of the AVTM MetroBus service. All retained and newly planted materials shall be maintained for five years from the first use of any part of the road by the public and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the Local Planning Authority gives written consent to any variation.

Reason: To ensure a coordinated design of the elements identified so as to ensure the satisfactory appearance and functioning of the development, in the interest of the protecting and enhancing the character of the site and the area and to ensure its appearance is satisfactory."

Condition 13**Construction Phases**

The construction of the development hereby approved shall not proceed other than in accordance with the approved Works Programme Phasing Plan (drawing ref: 201749-PA-52 P4, received 8 January 2016) unless the Local Planning Authority gives written consent for any variation.

Reason: It is necessary that the stages of development and the provision of associated infrastructure follow a co-ordinated sequence and in order to minimise construction impacts and to enable conditions to be discharged for parts of the scheme to facilitate the sequencing of the approval of further details and construction.

The removal of Condition 4 is sought on the basis that the number of trees proposed to be removed as part of the extant planning permission has been reduced and the opportunities to replace these trees have been limited for reasons of design, safety and the suitability of the locations for planting. A number of trees were also proposed to be planted as enhancements beyond the requirements of the Bristol Tree Replacement Standard.

The following tree removals and replacements were proposed, have been removed, and are now proposed as part of this application:

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Application Ref. / Stage	Tree Removal	Tree Replacements Required by BTRS	Tree Planting (Total)
13/05648/FB (as amended by 16/05853/X)	11 (proposed)	30	41 (proposed)
Scheme Implementation	7 (actual removed)	15	4 (actual planted)

Accounting for the four replacement trees provided to date, 11 further trees are subsequently required for mitigation in line with DM17. In addition, 13 trees would be provided as enhancements.

In line with the Planning Obligations SPD, if new planting cannot be provided onsite, a contribution shall be made towards new planting on public land. Subject to grant of planning permission, a Memorandum of Understanding (MoU) would be required for a payment for tree compensation of £41,348.07 (plus any index-linked increases) for the 24 replacement trees.

The removal of Conditions 10 and 13 are sought to, in effect, remove the Avon Crescent element of the AVTM scheme from the extant permission ref. 16/05853/X. Condition 13 is suggested by the Applicant as no longer being required as construction of the extant AVTM scheme has completed (with the exception of Avon Crescent).

RESPONSE TO PUBLICITY AND CONSULTATION

COUNCILLOR COMMENTS

Concerns were raised that initially when the application was consulted upon, there was no local councillor in place to represent the Hotwells and Harbourside Ward. To allow for councillor involvement, the application was held into abeyance until a new Ward Member was appointed.

The application was subsequently referred to Committee by Councillor McAllister for the following reasons:

“The proposal will result in a substantial increase in traffic, with associated noise and danger, along a residential street.

Avon Crescent has become a much-utilised walking and cycling route, and removing it from this use now in the face of near-universal public opposition would be detrimental on grounds of air quality and public health and amenity.”

PUBLIC COMMENTS

One round of public consultation was undertaken on this application.

In response, 26 comments were received from 21 different addresses. All were in objection apart from one comment in support, however this was a duplicate of an objecting comment and so should not be considered as a comment in support.

The following planning issues were raised in objection to the scheme:

- Concerns that there would be a negative impact on road safety, in particular in relation to pedestrians and cyclists using the route.
- Concerns about an increase in traffic from current levels and the lack of traffic calming measures.

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- Concerns that there would be a negative impact on road safety as vehicles using this route would speed and rat-run, and existing footways are insufficient.
- Concerns that the removal of the turning-head would require vehicles to conduct a three-point-turn into moving traffic.
- Concerns that the claim of reduced traffic levels has not been evidenced and that there are no Environmental or Traffic Impact Reports provided with the application.
- High numbers of pedestrians and cyclists benefit from this route and so shared space is necessary.
- Concerns that the payment for landscaping would not be spent in the local area, and that the trees would be planted elsewhere in the city.
- Concerns that allowing vehicles to drive along Avon Crescent would have a detrimental impact on Bristol's heritage and the Conservation Area.
- Stating that the mitigation now proposed to no longer be required was a key reason many people supported the original application.
- Concerns that there has been no consultation with local residents.
- Concerns that the plans provided are inadequate.
- Concerns that this would set precedent for developers that they do not have to fulfil safety and environmental mitigation conditions, and would undermine public trust.
- Concerns that the funding for mitigation works has not been spent.
- Concerns that the applicant is in breach of the original planning consent by not fulfilling these conditions.

Councillor McAllister and Councillor Townsend submitted consultation responses in objection to the proposed scheme. Their responses raised the following concerns:

- The route is fully integrated for walking and cycling, and this would be removed should traffic be reintroduced.
- There is no evidence for traffic reduction.
- There would be an increase in noise and traffic and decrease in road safety.
- The proposals would discourage the use of sustainable transport.
- The trees should be planted as originally proposed, especially given there is a climate emergency.

Three local groups also responded to the public consultation; their responses are as follows:

Bristol Tree Forum

- Removing the requirement to plant replacement trees is against local planning policy which requires this and does not allow payment as an alternative.
- The contribution towards tree planting citywide is unclear and planning policy requires trees be planted within a one-mile radius and within the same Area Committee.
- Permanent removal of tree cover should not be a delegated decision and should be considered by the Development Committee.
- The application should not be approved in an incremental manner as this undermines the decision making, and the decision should be considered by the Development Committee not as a delegated decision.
- There is no justification for reducing the number of trees from 55 to 24.
- The compensation amount is incorrect and should be £182,538.40 (indexed £267,643.77).
- The closing date for comments was the day before a byelection in the ward and so there is not proper representation for the ward regarding this decision.
- The title of the planning application does not reflect its intent and so makes it harder for the public and stakeholders to engage.

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- The applicant is in breach of the original planning consent as details for replacement trees should have been approved by the LPA before commencement of works.
- Condition 5 of the original planning consent requires a Detailed Arboricultural Method Statement and Arboricultural Supervision of Key Works; these have not been submitted.

Bristol Walking Alliance

- While the shared space proposal is no longer acceptable, the temporary restrictions in place should be made permanent to turn Avon Crescent into a cul-de-sac for motor vehicles.
- There are insufficient footways and road crossing infrastructure in place.
- Previous application 18/02968/X included traffic calming and crossing measures but was refused for being harmful to conditions of highway safety. The current application does not include traffic calming or crossing measures.
- The tree planting is required due to concerns regarding air quality and lack of shade.

Bristol Cycling Campaign

- The closure of the southern end of Avon Crescent has made the route more pleasant and it is a key walking and cycling route, especially as it is part of the National Cycle Network.
- It is government policy to enable walking and cycling, this application does not align with this.
- The existing footways are insufficient for the amount of pedestrian traffic.
- The ward is currently not represented by a councillor and the application can only be found by searching for Carter Road, in Bishopsworth.
- Concerns whether local residents and Active Travel England have been consulted fully.
- The application would enable driving, contrary to the One City Plan and tackling climate change.
- The temporary closure should be made permanent as it works well.

INTERNAL CONSULTATION

Transport Development Management – No objection

“When the TTRO ends [*note – the TTRO has now expired*], the road would revert to its original design, a through route eastbound with a no entry for westbound traffic at the eastern end of the road. The road operated in this fashion prior to the TTRO closure and is an existing highway layout.

The northern end of Avon Crescent remains how it is now which is how it was before the TTRO. Avon Crescent is a two-way road not a one way road, there is just a point no entry at the eastern end of the road. This would not be a planning issue.

For the pedestrian, the road is going back to operate how it operated previously when the TTOR is removed so pedestrians would be expected to behave as they behaved previously.

Removing the condition to implement a shared space is accepted on the basis it is no longer accepted practice as per government guidance.”

Arboricultural Officer – No objection – Surgery Item

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“The reduction in the loss of trees as part of the AVTM scheme is supported and the challenges around providing the full number of replacements set out in the 2016 scheme are understood. Given that the trees lost were within the highway or its vicinity, the payment trigger would be preferred for a tree in hardstanding as set out in the Planning Obligations Supplementary Planning Document.”

Following discussion with the applicant and a review of Appendix 2 of the Arboricultural Impact Assessment and Tree Protection Plans (Bosky Trees, November 2013), it was agreed that a payment for a mix of trees in soft landscaping and hardstanding would be provided on the basis that this is consistent with the permission ref. 13/05648/FB.

KEY ISSUES**A. WOULD THE PROPOSED REMOVAL OF CONDITION 4 RESULT IN ANY UNACCEPTABLE IMPACTS UPON GREEN INFRASTRUCTURE?**

Policy BCS9 states that individual green assets should be retained wherever possible and integrated into new development.

Policy DM17 requires development to integrate important existing trees.

Condition 4 has been applied *“To ensure that the development provides adequate mitigation for the loss of the trees on the site and complies with the Bristol Tree replacement Standards.”*

The following tree removals and replacements were proposed as part of the extant permission. It is noted that Condition 4 refers to 55 trees, however it is unclear where this figure has been derived from and is assumed to be an error. The following is set out in the approved Arboricultural Impact Assessment (Bosky Trees, November 2013):

Application Ref.	Tree Removal	Tree Replacements Required by BTRS	Tree Planting (Total)
13/05648/FB (as amended by 16/05853/X)	11 (proposed)	30	41 (proposed)

Delivery of the extant permission ref. 16/05853/X to date has managed to retain existing trees at Spike Island (T10) and Bedminster (T53, T54 and T55). This is in accordance with Policy BCS9, which seeks to retain trees whenever possible.

The applicant has provided justification in relation to the lack of tree planting on Avon Crescent. It has identified that the proposed tree replacements would conflict with the proposal to return Avon Crescent to the previous road layout (considered in detail in Key Issue B below).

As tree replacements cannot be provided onsite, a contribution to off-site planting has been agreed in principle, subject to this application gaining consent. This contribution complies with the Bristol Tree Replacement Standard set out in Policy DM17 and the payments set out in the Planning Obligations Supplementary Planning document. A payment based on a mixture of trees being provided in open ground and trees in hardstanding is proposed based on the results of the Arboricultural Impact Assessment (Bosky Trees, 2013).

Application Ref.	Tree Removal	Tree Replacements Required by BTRS	Tree Planting (Total)
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22/05943/X (this application)	7 (actual removed)	15	4 (actual planted) 24 (contribution towards off-site planting): 11 trees for mitigation, and 13 trees for enhancement.
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This payment would be in excess of the requirements of the Bristol Tree Replacement Standard and would provide an opportunity for the provision of trees off-site to enhance the character of the surrounding area in appropriate locations.

In the public statements to Committee on 10 May 2023, a query was raised as to the size of the contribution sought and why it represented the costs for both a mix of trees in hardstanding and in open ground. The proposed mix of contributions sought is set out below:

	Cost per tree	No. of trees	Total
Open Ground	£765.21	15	£11,478.15
Hardstanding	£3,318.88	9	£29,869.92
Total			£41,348.07

A mix of contributions for trees in hardstanding and in open ground was considered appropriate based on the tree planting proposed in Appendix 2 of the Arboricultural Impact Assessment from permission ref. 16/05853/X¹. This sets out 24 of 41 replacement trees are planted in open ground, equivalent to approximately 60%. As such, approximately 60% of the trees for the contribution would be planted in open ground.

On the basis of the above, it is concluded that the proposed amendments would comply with the reasons set out in Condition 4 and on the basis that a payment is made in accordance with the Bristol Tree Replacement Standard, the condition is no longer required.

It is considered that the proposed removal of condition 4 would not result in any unacceptable impacts upon green infrastructure and would accord with Policy BCS9 by retaining more trees than the extant consent.

B. WOULD THE PROPOSED REMOVAL OF CONDITIONS 10 AND 13 RESULT IN ANY UNACCEPTABLE IMPACTS?

Policy BCS10 states that developments should be designed and located to ensure the provision of safe streets. Development should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

Policy BCS21 of the Core Strategy aims to ensure that all new development in Bristol achieves high standards of urban design. The policy states that design can contribute positively to local

¹ The approved Arboricultural Impact Assessment for AVTM is saved under 13/05648/FB as the s73 permission ref. 16/05853/X did not affect tree removals.

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character by responding to the underlying landscape structure, distinctive patterns and forms of development.

Policy BCS21 of the Core Strategy states that high quality design should consider the amenity of both existing and future development.

Policy BCS22 states that development proposals will safeguard or enhance heritage assets [including listed buildings and Conservation Areas] and the character and setting of areas of acknowledged importance. Avon Crescent is comprised of Grade II Listed Georgian terraces, located within the City Docks Conservation Area.

Policies BCS23, DM33 and DM35 state that development should be sited and design in a way to avoid adverse impacts on environmental amenity by reason of pollution including: noise, vibration and air quality.

Policy DM23 of the Site Allocations and Development Management Policies outlines that development should not give rise to unacceptable traffic conditions and will be expected to provide safe and adequate access onto the highway network.

Condition 10 was attached to 16/05853/X to: *“ensure a coordinated design of the elements identified so as to ensure the satisfactory appearance and functioning of the development, in the interest of the protecting and enhancing the character of the site and the area and to ensure its appearance is satisfactory.”*

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This remainder of this Key Issue covers each aspect of the condition reason below in turn.

Appearance and character

The condition was applied on the basis that it was necessary to protect and enhance the character of the site, in accordance with Policy BCS22. When considering the Policy wording, there are two options for development proposals within Conservation Areas. The Policy states that development must safeguard (or protect) **or** [emphasis added] enhance the heritage asset and its setting, in this case the City Docks Conservation Area.

By returning to an existing road layout and function, the scheme would not enhance the character of the Conservation Area. Whilst this would be a reduction in terms of the benefits of the extant scheme, the Policy requires as a minimum that the appearance of the area is maintained and the proposal not to implement a scheme at Avon Crescent would be consistent with this.

By maintaining the existing road layout, there is no 'new development' as set out in Policy BCS21 to assess in terms of high-quality urban design.

It is considered that the proposed removal of the Avon Crescent element of AVTM and Condition 10 would accord with Policy BCS22 and safeguard the character of the City Docks Conservation Area.

Function of the development and co-ordinated design

The application sets out that MetroBus AVTM scheme does not require the inclusion of the Avon Crescent section of the scheme as approved via application 13/05648/FB to function.

This is evidenced by the fact that the MetroBus 'm2' service, approved in the Ashton Vale to Temple Meads and Bristol City Centre Rapid Transit Order (the Order) and subsequently amended via 13/05648/FB and 16/05853/X has been running since September 2018 in the absence of a scheme on Avon Crescent.

It is also acknowledged that there is no requirement within the Town and Country Planning Act 1990 for planning permissions to be completed in full to remain valid.

The applicant has set out that shared space would not be an appropriate intervention based on government guidance in the Inclusive Transport Strategy (Department for Transport, 2018). This has been reviewed by Transport Development Management and it is agreed that given the location of Avon Crescent and its layout on a bend, it would not be an acceptable intervention in light of this guidance.

Queries were raised by the Local Planning Authority in respect of highway safety and whether a Road Safety Audit may be appropriate. Transport Development Management confirmed that such an assessment cannot be required as based on the Design Manual for Roads and Bridges (DMRB) and the guidance from Chartered Institution of Highways & Transportation. This states that a Road Safety Audit can only be required when a scheme is 'likely to change road user behaviour'. As this scheme is returning to an existing road layout, the way the road functions and its safety cannot be considered as a planning issue.

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It is also acknowledged that the continued closure of Avon Crescent is not an option as a TTRO can only be applied for twice for a period of up to 18 months, and it cannot be re-applied in the case of Avon Crescent.

Transport Development Management has raised no objection to the application, and it is considered that the function of the MetroBus service would not be impeded by the deletion of Condition 10.

Construction Impacts

Condition 13 was applied to 16/05853/X to ensure that *“the stages of development and the provision of associated infrastructure follow a co-ordinated sequence and in order to minimise construction impacts and to enable conditions to be discharged for parts of the scheme to facilitate the sequencing of the approval of further details and construction.”*

The proposed non-implementation of Avon Crescent would not require any further construction. As such, no impacts are anticipated upon residential amenity through construction.

Concerns have been raised by interested parties in respect of increase noise and air pollution as a result of the removal of Conditions 10 and 13 and the non-implementation of Avon Crescent. The effect of an existing road layout on residential amenity is not a planning issue and cannot be considered as part of this application.

As the construction of the AVTM scheme has been completed in a co-ordinated and sequenced manner, and there is no development proposed to generate construction impacts, it is considered that Condition 13 is no longer required, and it can be deleted.

CONCLUSION

The proposed approach to retain more existing green infrastructure and provide a contribution to off-site tree replacements would be consistent with Policies BCS9 and DM17, as well as the Planning Obligations SPD.

Whilst the extant scheme for Avon Crescent (ref. 16/05853/X) would provide an enhancement to the character of the Conservation Area, concerns have been raised by Transport Development Management about the safety of the proposals. The proposal to retain the existing road layout at Avon Crescent would be consistent with Policy BCS22 by conserving the character of the Conservation Area.

The scope of considerations in respect of Transport and Highways and Residential Amenity are extremely limited given that safety of an existing road layout and amenity impacts arising from it are not planning issues.

There is no reason why Conditions 4, 10 and 13 should be retained and as such, it is recommended that the planning application is approved, subject to planning agreement and conditions.

The following plans are recommended to be removed from the consent:

- 201749-PA-522 P5 Construction Phase Plan, received 24 November 2017
- R06-01 T1 Avon Crescent retaining wall, received 17 December 2013
- 201749-PA-316 P1 Landscape proposals Avon Crescent, received 17 December 2013

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- 201749-PA-202 P3 Proposed general arrangement sheet 2, received 17 December 2013

PLANNING AGREEMENT

As BCC cannot enter into a legal agreement with itself, the contribution towards tree mitigation is proposed to be secured via an internal memorandum of understanding (MoU). The MoU is to secure the following terms:

- Payment towards 24 replacement trees at a cost of £41,348.07.

RECOMMENDED – Resolution to GRANT planning permission, subject to delegation to officers to finalise planning agreement.

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CONDITIONS**List of approved plans**

1. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

201749-PA-01C Red site location plan (1 of 9), received 17 December 2013
 201749-PA-02C Red site location plan (2 of 9), received 17 December 2013
 201749-PA-03C Red site location plan (3 of 9), received 17 December 2013
 201749-PA-04C Red site location plan (4 of 9), received 17 December 2013
 201749-PA-05C Red site location plan (5 of 9), received 17 December 2013
 201749-PA-06C Red site location plan (6 of 9), received 17 December 2013
 201749-PA-07C Red site location plan (7 of 9), received 17 December 2013
 201749-PA-08C Red site location plan (8 of 9), received 17 December 2013
 201749-PA-09C Red site location plan (9 of 9), received 17 December 2013
 201749-PA-201 P3 Proposed general arrangement sheet 1, received 17 December 2013
 201749-PA-209 P3 Proposed general arrangement sheet 9, received 17 December 2013
 201749-PA-210 P3 Proposed general arrangement sheet 10, received 17 December 2013
 201749-PA-211 P3 Proposed general arrangement sheet 11, received 17 December 2013
 201749-PA-302 Landscape proposals cumberland road, received 17 December 2013
 201749-PA-317-319 P2 Landscape proposals Redclif Hill, received 17 December 2013
 201749-PA-512 P1 Extent of demolition, received 17 December 2013
 201749-PA-516 Bus stop detail 1 of 2, received 17 December 2013
 201749-PA-517 P1 Bus stop detail 2 of 2, received 17 December 2013
 201749-PA-518 P2 Temporary construction compounds, received 17 December 2013
 R05-04 T1 Cumberland road wall sections 1 of 2, received 17 December 2013
 R05-05 T1 Cumberland road wall sections 2 of 2, received 17 December 2013
 R05-06 T1 Cumberland Road wall railings, received 17 December 2013
 AVTM-PA-501 Cross section chainage 3275m, received 17 December 2013
 AVTM-PA-502 P1 Cross section chainage 3350m, received 17 December 2013
 AVTM-PA-503 P1 Cross section chainage 3400m, received 17 December 2013
 AVTM-PA-504 P1 Cross section chainage 3550m, received 17 December 2013
 AVTM-PA-505 P1 Cross section chainage 3750m, received 17 December 2013
 AVTM-PA-506 P1 Cross section chainage 4220m, received 17 December 2013
 AVTM-PA-507 P1 Cross section chainage 4950m, received 17 December 2013
 AVTM-PA-508 P1 Cross section chainage 4980m, received 17 December 2013
 AVTM-PA-509 P1 Cross section chainage 5000m, received 17 December 2013
 AVTM-PA-510 P1 Cross section chainage 5275m, received 17 December 2013
 AVTM-SK-12 P1 Commercial Road flood protection, received 17 December 2013
 AVTM-SK-13 P1 Commercial Road flood protection sections, received 17 December 2013
 AVTM-X-GA-SK32 Landscape proposals Wapping Wharf, received 17 December 2013
 201749-PA-203 P3 Proposed general arrangement sheet 3, received 17 December 2013
 201749-PA-204 P3 Proposed general arrangement sheet 4, received 17 December 2013
 201749-PA-205 P3 Proposed general arrangement sheet 5, received 17 December 2013
 201749-PA-206 P3 Proposed general arrangement sheet 6, received 17 December 2013
 201749-PA-207 P3 Proposed general arrangement sheet 7, received 17 December 2013
 201749-PA-208 P3 Proposed general arrangement sheet 8, received 17 December 2013

Development Control Committee B – 18 October 2023**Application No. 22/05943/X : (Bathurst Basin Bridge Commercial Road) Land Between The A370 Long Ashton Bypass In North Somerset And Cater Road Roundabout Cater Road Bristol**

Environmental Statement Volume 2, Chapter 10 Nature Conservation – November 2013, received 17 December 2013

DH0245-C001 D Redcliff Hill inbound Site clearance, received 9 March 2015

DH0245-C002 D Redcliff Hill Inbound Kerbs and Surfaces + turning head tracking drawing, received 9 March 2015

DH0245-C003 D Redcliff Hill inbound Drainage and ducting, received 9 March 2015

DH0245-C004 B Redcliff Hill Signing, received 2 December 2014

DH0245-C005 B Redcliff Hill Inbound Road markings, received 2 December 2014

DH0245-C007 D Redcliff Hill inbound Construction Drawings General Arrangement, received 9 March 2015

DH0245-C009 Redcliff Hill Tree Protection Plan, received 2 December 2014

DH0245-C011 C Site clearance Redcliffe Roundabout, received 9 March 2015

DH0245-C012 C Kerbs and surfaces Redcliffe Roundabout, received 9 March 2015

DH0245-C013 C Ducts and drainage Redcliffe Roundabout, received 9 March 2015

DH0245-C014 C Road markings and levels Redcliffe Roundabout, received 9 March 2015

DH0245-C015 C Signing drawing Redcliffe Roundabout, received 9 March 2015

DH0245-C016 C Cross sections around central island Redcliffe Roundabout, received 9 March 2015

DH0245-C017 C General Arrangement Redcliffe Roundabout, received 9 March 2015

DH0245-C111 Redcliff Hill Outbound Site Clearance, received 2 December 2014

DH0245-C113 Redcliff Hill Outbound Road marking and setting out, received 2 December 2014

DH0245-C114 Redcliff Hill Outbound General Arrangement, received 2 December 2014

DH0245-C141 Commercial Road Signs, road markings and tree pit, received 2 December 2014

DH0245-C142 Commercial Road General Arrangement, received 2 December 2014

Use and Supply of Construction Materials Planning condition 8, received 2 December 2014

Arboricultural Method Statement Planning condition 7 (ii), received 2 December 2014

GAV TMR-SK-033 Bathurst Basin Bridges Street Lighting 50% Lux Contour Plot, received 23 February 2015

Written Scheme of Investigation for a Programme of Archaeological Work, received 23 February 2015

Construction Environmental Management Plan Planning Condition 3, received 9 March 2015

370125 8 Bay Landmark Plate MK1A, received 26 June 2015

370127 6 Bay Landmark MK1a Plate Roof - Metrobus, received 26 June 2015

Materials Management Plan (MMP) Form Section 2 & Section 3 Ashton Vale to

Temple Meads Rapid Transport Scheme, received 4 November 2015

201749-PA-209 P4 Planning Application General Arrangement Sheet 9 of 11, received 6 November 2015

201749-PA-210 P4 Planning Application General Arrangement Sheet 10 of 11, received 6 November 2015

E14067-C111 A Redcliff Hill Southbound Site Clearance, received 6 November 2015

E14067-C112 A Bedminster Bridge works Kerbs & Surfaces, received 6 November 2015

E14067-C113 A Bedminster Bridge works Ducts & Drainage, received 6 November 2015

E14067-C115 A Redcliff Hill Southbound Road markings & Setting Out, received 6 November 2015

E14067-C150 A Redcliff Hill southbound works General arrangement, received 6 November 2015

MET_AVTM_013 0 Cumberland Road / Bus Link Rd Traffic Signal General Arrangement, received 8 January 2016

TBC-1 Bristol Bus Route Railing Detail, received 1 February 2016

C12149 Site Management Plan, received 18 April 2016

AVTM-3-GA-DRG-100 T5 Bathurst Basin Bridges Site Clearance, received 27 May 2016

Development Control Committee B – 18 October 2023**Application No. 22/05943/X : (Bathurst Basin Bridge Commercial Road) Land Between The A370 Long Ashton Bypass In North Somerset And Cater Road Roundabout Cater Road Bristol**

AVTM-3-GA-DRG-102 T5 Bathurst basin Bridges Pavements, Kerbs and Railing Details Sheet 2 of 2, received 27 May 2016

AVTM-3-DRG-B02-014 C3 Bathurst Basin Bridges New Bridge Commercial Road River Wall, received 27 May 2016

AVTM-3-GA-DRG-101 C2 Bathurst Basin Bridges Pavements, Kerbs and Railing Details Sheet 1 of 2, received 27 October 2016

AVTM-3-GA-DRG-103 C2 Bathurst Basin Bridges Highway Setting Out Details Sheet 1 of 2, received 27 October 2016

AVTM-3-GA-DRG-104 C1 Bathurst Basin Bridges Highway Setting Out Details Sheet 2 of 2, received 27 October 2016

AVTM-3-GA-DRG-105 C2 Bathurst basin Bridges Highway Cross Sections Sheet 1 of 2, received 27 October 2016

AVTM-3-GA-DRG-106 C1 Bathurst Basin Bridges Highway Cross Sections Sheet 2 of 2, received 27 October 2016

AVTM-3-GA-DRG-107 C2 Bathurst Basin Bridges Road Markings and Traffic Signs Sheet 1 of 2, received 27 October 2016

AVTM-3-GA-DRG-108 C1 Bathurst Basin Bridges Road Markings and Traffic Signs Sheet 2 of 2, received 27 October 2016

AVTM-3-GA-DRG-109 C2 Bathurst Basin Bridges Street Lighting, Ducting and Drainage Sheet 1 of 2, received 27 October 2016

AVTM-3-GA-DRG-110 C2 Bathurst Basin Bridges Street Lighting, Ducting and Drainage Sheet 2 of 2, received 27 October 2016

AVTM-3-GA-DRG-111 C2 Bathurst Basin Bridges Surface Water Drainage Inspection Chamber Details, received 27 October 2016

AVTM-3-TPP-DRG-001 C2 Bathurst Basin Bridges Tree Protection Plans Sheet 1, received 27 October 2016

AVTM-3-TPP-DRG-002 C2 Bathurst Basin Bridges Tree Protection Plans Sheet 2, received 27 October 2016

AVTM-3-TPP-DRG-003 C2 Bathurst Basin Bridges Gods Garden Tree Planting, received 27 October 2016

AVTM-3-DRG-B02-008 C2 Bathurst Basin Bridges New Bridge Bridge Deck Reinforcement Details, received 27 October 2016

AVTM-3-DRG-B02-013 C2 Bathurst Basin Bridges New Bridge Gods Garden Access Steps, received 27 October 2016

AVTM-3-ST-DRG-B02-001 C4 Bathurst Basin Bridges New Bridge General Arrangement, received 27 October 2016

AVTM-3-ST-DRG-B02-002 C6 Bathurst Basin Bridges New Bridge Site Limits & Site Clearance, received 27 October 2016

AVTM-3-ST-DRG-B02-003 C4 Bathurst Basin Bridges New Bridge Work Phases, received 27 October 2016

AVTM-3-ST-DRG-B02-004 Bathurst Basin Bridges New Bridge Pile Details, received 27 October 2016

AVTM-3-ST-DRG-B02-005 C3 Bathurst basin Bridges New Bridge Steelwork Layout, received 27 October 2016

AVTM-3-ST-DRG-B02-006 C2 Bathurst basin Bridges New Bridge Steelwork Details, received 27 October 2016

AVTM-3-ST-DRG-B02-007 C4 Bathurst Basin Bridges New Bridge Concrete Outline, received 27 October 2016

AVTM-3-ST-DRG-B02-009 C2 Bathurst Basin Bridges New Bridge End Screen Reinforcement Details, received 27 October 2016

Development Control Committee B – 18 October 2023

Application No. 22/05943/X : (Bathurst Basin Bridge Commercial Road) Land Between The A370 Long Ashton Bypass In North Somerset And Cater Road Roundabout Cater Road Bristol

AVTM-3-ST-DRG-B02-010 C3 Bathurst Basin Bridges New Bridge Waterproofing and General Details, received 27 October 2016

AVTM-3-ST-DRG-B02-011 C2 Bathurst Basin Bridges New Bridge SE Retaining Wall General Arrangement, received 27 October 2016

AVTM-3-ST-DRG-B02-012 C2 Bathurst Basin Bridges New Bridge SE Retaining Wall General Arrangement, received 27 October 2016

AVTM-3-ST-DRG-B02-015 C3 Bathurst Basin Bridges Northeast and Southwest Wingwalls General Arrangement, received 27 October 2016

AVTM-3-ST-DRG-B02-016 C3 Bathurst Basin Bridges New Bridge Wingwall Reinforcement Details, received 27 October 2016

AVTM-3-ST-DRG-B02-017 C2 Bathurst Basin Bridges North Abutment Stub Wall, received 27 October 2016

287587A-HHE-300-008 P2 FENCING 8 OF 9, received 20 March 2017

287587A-HHE-301-001 P2 FENCING STANDARD DETAILS, received 20 March 2017

287587A-HHE-301-002 P2 FENCING STANDARD DETAILS, received 20 March 2017

287587A-HHE-301-004 P2 FENCING STANDARD DETAILS, received 20 March 2017

287587A-HHE-500-108 P3 DUCTING LAYOUT 8 OF 10 , received 20 March 2017

287587A-HHE-1100-008 P5 KERBING AND FOOTWAY LAYOUT 8 OF 10, received 20 March 2017

287587A-HHE-1101-001 P2 KERBING AND FOOTWAY STANDARD DETAILS 1 O F 2, received 20 March 2017

287587A-HHE-1101-002 P2 KERBING STANDARD DETAILS, received 20 March 2017

287587A-HHE-1101-003 P1 KERBING STANDARD DETAILS, received 20 March 2017

287587A-HHE-1201-001 P1 BOLLARD AND SIGNING STANDARD DETAILS, received 20 March 2017

287587A-HHE-1200-008 P2 TRAFFIC SIGNS AND ROAD MARKINGS, received 20 March 2017

287587A-HHE-1200-009 P2 TRAFFICS SIGNS AND ROAD MARKINGS 9 OF 10, received 20 March 2017

287587A-HHE-4000-002 P3 BUS STOP ARRANGEMENTS CREATE CENTRE IN AND OUT BOUND, received 20 March 2017

AVTM-3-GA-DRG-101 REV C4 BATHURST BASIN BRIDGES PAVEMENTS, KERBS AND RAILING DETAIL 1 OF 2, received 7 September 2017

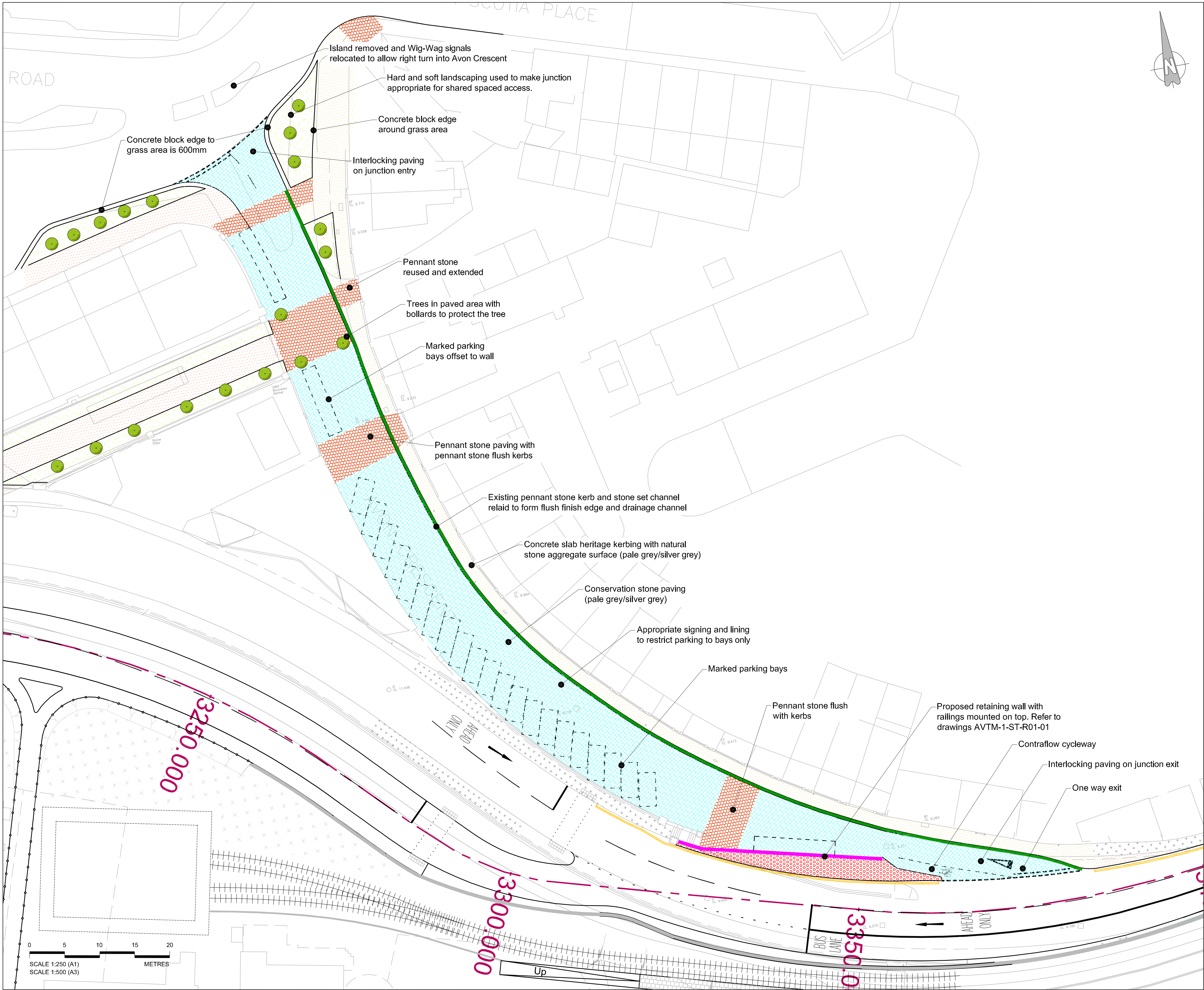
AVTM-3-GA-DRG-107 REV C5 BATHURST BASIN BRIDGES ROAD MARKINGS AND TRAFFIC SIGNS SHEET 1 OF 2, received 7 September 2017

EX18055-EX100, Existing Layout Survey Information, received 15 December 2022.

Reason: For the avoidance of doubt.

Supporting Documents

1. **(Bathurst Basin Bridge Commercial Road) Land Between The A370 Long Ashton Bypass In North Somerset And Cater Road Roundabout Cater Road**
 1. Extant Scheme – 201749-PA-316 Avon Crescent
 2. Proposed scheme – Revert Back to Previous Layout
 3. Arboricultural Note



Notes:

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General Notes:

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2. Do not scale from this drawing
3. Horizontal and vertical alignment is based upon Topographical Survey dated October 2009

Key:

- Retaining wall
- Existing footways
- Concrete slab heritage paving with natural stone aggregate surface (pale grey/silver grey)
- Grass Mix A (verge)
- Pennant Stone paving.
- Raised conservation stone paving (pale grey/ silver grey) set flush with adjacent footway
- Bitumen surface footpath
- Stone set channel
- Proposed Tree
- Conservation stone pedestrian deterrent paving

Rev	By	Chkd	Apprvd	Date	Description

Client

West of England

Bath & North East Somerset Council, North Somerset Council, South Gloucestershire Council

travel

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Project **BRISTOL METROBUS ASHTON VALE to TEMPLE MEADS and BRISTOL CITY CENTRE**

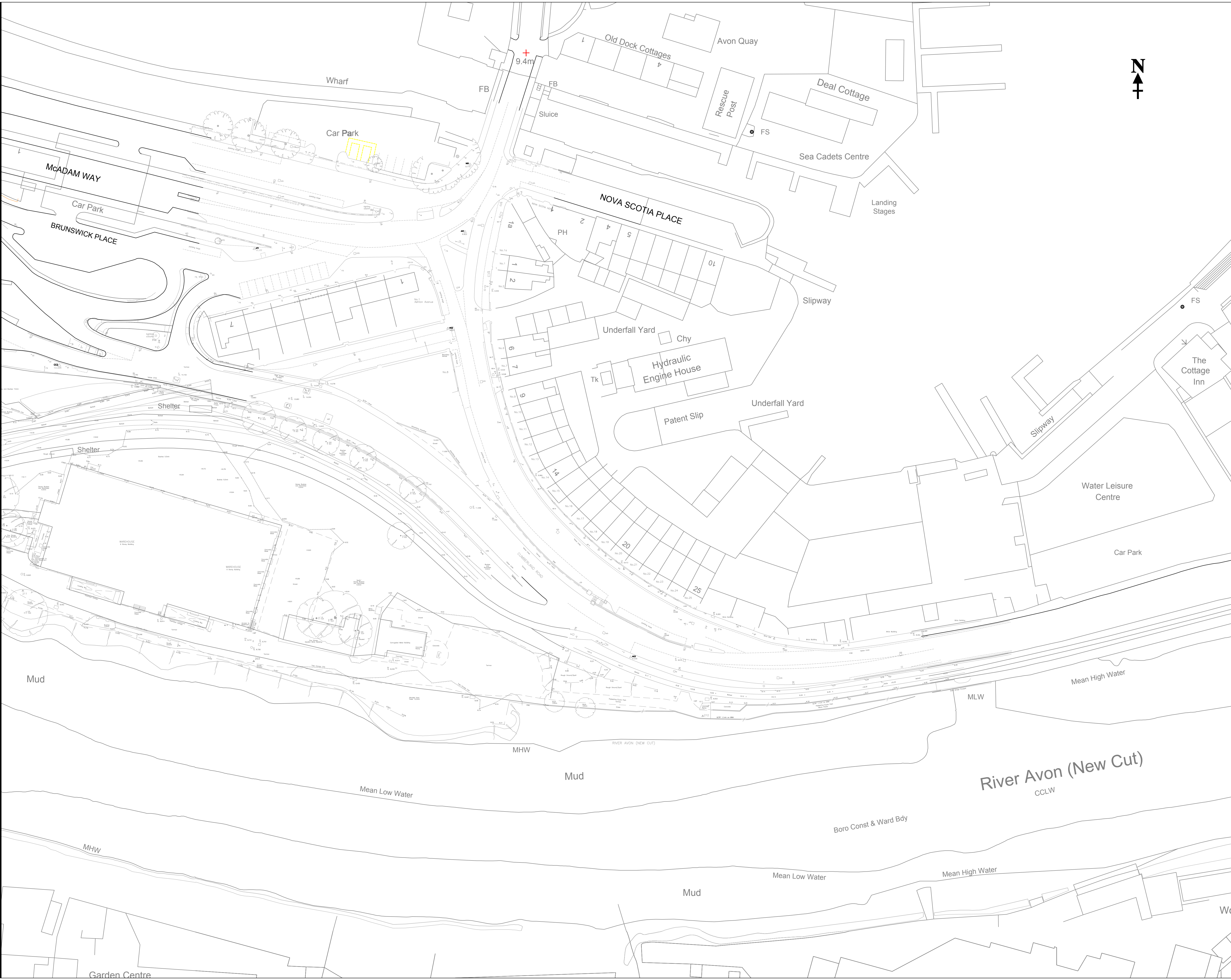
Drawing **AVON CRESCENT SHARED SPACE DETAIL**


Drawn by: JA	Date: 04/10/2013
Checked by: PF	Date: 13/11/2013
Approved by: BS	Date: 29/11/2013

Drawing No.	Revision
201749-PA-316	P1

Drawing Scale: 1:250 @ A1

Consent this work & name
Reference file path
User and Plot Date





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Rev.	Date	Description	By

Project

Avon Crescent Highway Improvements

Title

Existing Layout Survey Information

Client

Transport
Growth and Regeneration
Mark Sperry

Drawn by

Checked by

Date Drawn

Date Issued

08/04/2020

Status

Issued by

ND

Scale

1:500 @ A1

Project Number

E18055 - EX100 -

Drawing

Revision

TREE PLANTING AVTM REVISED SECTION OF THE AVTM FROM CUMBERLAND ROAD JUNCTION TO REDCLIFFE ROUNDABOUT

Introduction

This note is written to provide the justification for a reduction in the levels of planting required on this section of the AVTM scheme and to seek to change the wording of the planning condition. Since the proposals were given consent, we have realised that not all of the initial trees which were earmarked for removal now need to be removed, reducing the number of trees that we should be obligated to plant as replacements and also the proportion of those required as enhancement trees required by Policy DM15. Given the nature of the route and comments from discussions with Highways and the Police there are also concerns with planting some of the trees on certain parts of the route – namely Avon Crescent.

Planning Condition

Condition 4 attached to the Planning Permission 16/05853/X (Application of variation of condition number 18 - Phase 1. for planning permission - 13/05648/FB) requires:

Submission and approval of replacement tree planting scheme

No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority details for 55 replacement trees to be planted in the approved locations, or alternative locations to be agreed by the Local Planning Authority. The details shall include the species, stock size, staking and guarding and establishment arrangements of each tree as well as a programme of works for the planting of the trees. The approved scheme shall be implemented so that planting can be carried out during the first planting season following the commencement of the AVTM MetroBus service. The trees shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To ensure that the development provides adequate mitigation for the loss of the trees on the site and complies with the Bristol Tree replacement Standards.

Policy - BCC Tree Planting requirements to replace trees

The requirements for replacement mitigation trees are set out as follows, taken from the Bristol Tree Replacement Standard 2012:

Trunk Diameter of Tree lost to development (cm measured at 1.5m above ground level)	Number of Replacement Trees
Less than 15	0-1
15-19.9	1
20-29.9	2
30-39.9	3
40-49.9	4
50-59.9	5
60-69.9	6

70-79.9	7
80+	8

Justification for the condition

The comments received regarding trees in relation to the application are not available on the BCC site as they're removed as soon as an application is granted consent.

However, the Committee Report includes a summary of comments as follows:

BCC TREE OFFICER

- *Proposed tree planting meets the Bristol Tree Replacement Standard (BTRS). Confirmation that utilities searches have been carried out in order to verify the feasibility planting is recommended.*
- *Recommendation that size, stock, species of new trees and protection of retained trees are secured by condition. A detailed method statement regarding works around retained trees should also be conditioned.*

BRISTOL TREE FORUM:

- *Proposed replanting includes 11 more trees than the BTRS requirement.*
- *Loss of trees on Redcliffe Hill is regrettable as they provide important visual amenity value.*

"The impact of the proposal on trees has been assessed for each section of the route and is judged to be acceptable. The tree officer has confirmed that 30 replacement trees would be required to compensate for the 11 trees to be felled. The applicant is proposing to plant a total of 55 trees along or nearby the route, providing enhancement in addition to the compensation requirement.

Whilst a suitable number of trees are proposed nearby sites where existing trees would be removed, further "enhancement" planting tends to be concentrated to pockets such as Brunel Lock, Avon Crescent, Gods Garden and Wapping Wharf, rather than being more evenly distributed throughout the route. The Bristol Tree Forum has identified the loss of trees on Redcliffe Hill and there are no proposed replacements within the immediate vicinity.

The majority of trees proposed to be removed are street trees that make a contribution to street scape. With replacements generally proposed nearby, there is limited opportunity for further trees to be planted within prominent locations within the street. The larger landscaped areas to meet the replacement standard are therefore deemed to be acceptable. Officers are satisfied that all opportunities for new planting along the route have been identified, which is generally constrained due to highway requirements and underground services.

Overall the proposed 55 replacement trees are considered sufficient to provide replacement and help mitigate the impact of the scheme, enhance the public realm and contribute to the Council's aspirations to increase the canopy cover across the city.

In summary, the proposed works across the revised route are satisfactory. Where necessary designs are sensitive to designated heritage assets, preserving and enhancing the character and appearance of parts of the conservation areas, and protecting the setting of listed buildings and non-designated heritage assets. The principles of the landscaping proposals are acceptable although detailed plans and samples of certain surfaces will be required to be submitted and approved post decision. Suitable tree replacement has been provided which will ensure that losses are adequately compensated and,

in some cases, offer enhancement to the areas in which new planting would be located. The proposed creation of a shared space, a reduction in through traffic at Avon Crescent with associated landscaping is considered an appropriate approach to improving links across the area."

How many trees have/will be removed?

The table below is from Appendix 2 of the Arboricultural Impact Assessment and Tree Protections Plans document produced in November 2013 to support the Revised AVTM Application. The calculations have been made based on the Bristol Tree Replacement Standard 2012. This covers the mitigation element of the tree planting requirements.

Tree number	Location	Species	Stem Ø (mm)	BS5837 Category	Hard/soft	Number of Replacement Trees Required	Proposed Replacements Nearby
T10	Spike Island	Horse chestnut	759	C1	Hard	7	3 (hard) in street
T18	God's Garden	Sycamore	174	C1	Soft	1	5 (soft) in other locations within God's garden
T19	God's Garden	Hawthorn	180	C1	Soft	1	
T20	God's Garden	Sycamore	354	C1	Soft	3	
T21	God's Garden	Sycamore	376	C1	Soft	3	
T22	God's Garden	Rowan	93	C1	Soft	1	
T53	Bedminster	Black pine	380	A1	Soft	3	4 (soft) in roundabout
T54	Bedminster	Silver birch	245	B2	Soft	2	
T55	Bedminster	Silver birch	338	B2	Soft	3	
T148	Redcliff Hill	Norway maple	370	B2	Hard	3	2 (hard) in street
T149	Redcliff Hill	Norway maple	360	B2	Hard	3	
	Avon Crescent				Hard		12 (hard) in street
	B Bond/CREATE				Soft		15 (soft) Birch in planting area
11 trees Removed						30	41

What has been done to date?

- T10 Tree at Spike Island - this tree has not been removed, the bus stop position has been altered to avoid it. This reduced the overall number of replacement trees required by 7.
- God's Garden – Five trees removed, 9 are therefore required.
- T53-T55 Bedminster – These trees were not removed, and therefore the trees are not required as mitigation
- T148-149 Redcliffe Hill – 2 trees were removed. We have planted 4 trees so far to replace these and are required to plant 6, leaving 2 trees still required.
- Avon Crescent – No trees are being removed. The trees indicated here for planting are now considered unsuitable. They would be very close to the carriageway and a number of times a year an 80 tonne wagon transporting boats uses the road as its only access. The planting of trees would make this essential access impossible.

Summary

To date we have removed 7 trees. Using the table above to identify the specific trees and adding up the 'number replacement trees required' column next to the trees removed and the Bristol Tree Replacement Standards therefore we would need to provide 15 trees in lieu of these removals.

We have removed 7 trees (not the 11 originally planned).

The replacement rate for these 7 trees removed is to plant 15 trees.

We have already planted 4 of these 15 trees (nearby Redcliffe Hill).

This leaves 11 trees to plant in total for mitigation.

Given that the number of 'mitigation' trees has reduced from 30 to 15 (a reduction of 50%), we suggest that the number of 'enhancement' trees equals the equivalent proportion (a reduction of 50%, rounded to the nearest tree). This would mean that 13 'enhancement' trees are provided, instead of the consented 25.

The total number of trees required is therefore 28 (15 mitigation, 13 enhancement), less the 4 already planted, requiring provision to be made via funding for 24 trees.